

# **ARTICLE 8**

## **OFF-STREET PARKING AND PRIVATE DRIVEWAY STANDARDS**

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## 8.1. <sup>(2)</sup>GENERAL STANDARDS.

### 8.1.1. APPLICABILITY

**A.** The provisions of this Section shall apply to any application for Zoning Clearance or Site Plan approval.

1. New Development – Unless otherwise expressly stated in this section, the parking, loading, and access standards of this section shall apply to all new buildings constructed and all new uses established in all zoning districts.
2. Additions and Expansions – Unless otherwise expressly stated in this section, the parking, loading, and access standards of this section shall apply when an existing structure or use is expanded or enlarged, through the addition of floor area, dwelling units, seating capacity, or other unit of measurement used for establishing off-street parking and loading requirements. Additional off-street parking and loading spaces shall be required only to serve the enlarged or expanded area.
3. Change of Occupancy – Unless otherwise expressly stated in this section, off-street parking and loading facilities shall be provided for any change of occupancy or manner of operation that would result in a requirement for more parking and loading spaces than the former use. Additional parking or loading spaces shall be required only in proportion to the extent of the change.

### **B. Exemption.**

The standards of this Article 8 shall not apply to:

1. Detached single-family, duplex, or triplex dwellings on individual lots of record (except that such dwellings shall conform to the requirements of § 8.1.3);

**C.** Parking areas which constitute the Principal Use of a Site shall comply with the provisions of § 8.1.2(A) – 8.1.2(F) hereto, but not the minimum number of spaces as required in § 8.3.1.

### 8.1.2. <sup>(1)</sup>GENERAL DESIGN STANDARDS.

**A. Location** - Required off-street parking area(s) shall be provided on the same parcel as the principal structure or use, unless shared parking is

provided as set forth in § 8.3.1(I) of this Ordinance.

1. No parking spaces shall be allowed in a required street yard or buffer yard as required in Article 7.

**B. Landscaping** – Landscaping shall be required in accordance with Article 7 Landscaping and Buffering Standards.

**C. Exterior Lighting** - Lighting sources shall be designed and constructed so as to direct light away from public rights-of-way and residentially zoned or developed areas.

**D. Paving Required** - All required parking and vehicular traffic surfaces shall be graded for drainage in accordance with § 9.2 Stormwater Control and shall be surfaced with concrete or bituminous asphalt pavement except as allowed by § 8.1.2(E) and 8.1.2(F), below. Alternative materials may be approved by the Administrator. Alternative materials shall only be considered if such material(s) exhibits equivalent load bearing and wear characteristics as concrete or bituminous asphalt. In making such a determination, the Administrator may consult the Public Works Director or other persons with knowledge of paving materials. All surfaces shall be maintained in sound condition free of weeds, dust, trash and debris.

**E. Overflow Parking** - Overflow parking areas, event parking areas and/or low-traffic storage yards shall use turf. Overflow parking shall be defined as off-street parking in excess of the minimum required by this Ordinance which is designed not to be used more than ten (10) times per year. A low-traffic storage yard means a storage area generating less than 30 ADT (average daily trips).

**F. Paving Exemption for Assembly Uses** Paving of parking areas and access ways for assembly uses (churches, sports facilities, fairgrounds, etc.) may be waived if evidence is presented to the Administrator that these spaces will not be used regularly on a daily basis (will be used less than five times per week). Parking areas for which paving is waived shall maintain a gravel or crushed stone surface. The gravel must be at

least two (2) inches deep throughout the vehicular use area, except as permitted in Watershed Protection Overlay Districts in Article 4. All parking areas for which paving is waived shall meet the minimum requirements of Volumes I-C and V of the North Carolina State Building Code for Accessibility and for Fire Prevention. All parking lots shall be constructed with proper drainage.

**G. Overhang Protection** - Wheel or bumper guards or curbing shall be provided, located and arranged so that no part of any parked vehicle will extend beyond the boundaries of the parking space and into a pedestrian crossing area.

1. Except where a wall is constructed, a minimum six (6) inch high vertical concrete curb (or individual bumper guard) shall be constructed or installed so that no part of a vehicle extends beyond the property line.

**H. Striping Required** - Off-street parking areas, as required by this Ordinance, shall be striped in accordance with the dimensions as set forth in this Article 8.

**I. Backing Movements Prohibited.** Except for single-, two-, and three-family dwellings on individual lots, parking spaces and driveways shall be arranged to require ingress and egress from the lot to a public street by forward motion of the vehicle.

**J. Sight Triangles** - Sight Triangles for intersections of driveways and public streets shall be regulated in accordance with Appendix C of this Ordinance.

**K. Upfit to existing Public Street Required.** New multi-family and nonresidential projects shall be required to provide curb and gutter and sidewalks\* to adjacent public streets which provide access to the development, <sup>(1)</sup>except as where approved for an exception per Section C.1.8. Sidewalk, Curb, and Gutter Exception. Such improvements may be subject to road widening and other related improvements. Road widening, sidewalks, curb and guttering, utility relocations, and all other related improvements shall be the responsibility of the developer and/or owner. Additional right-of-way dedication may be required, as deemed necessary, to accomplish future road improvements as determined by the

City of Kannapolis Public Works Director. Design standards shall be subject to review and approval by the Public Works Director. Design standards for pedestrian upfits to state maintained roads shall be subject to review and approval by the Public Works Director, and the N. C. Department of Transportation.

\*Note that sidewalks shall not be required in the I-1 and/or I-2 zoning districts. However, this shall not prohibit the requirement for other roadway improvements (such as curb and gutter or additional right-of-way).

**L. Maintenance Standards** - Parking lot access roads and off-street parking areas shall be properly maintained in all respects. In particular, parking lot access roads and off-street parking area surfaces shall be kept in good condition (free from potholes, structural failures, etc.) as determined by the City of Kannapolis Public Works Director, or his/her designee, and parking space lines or markings shall be kept clearly visible and distinct.

**M. Fractions** - When calculation of the number of required parking spaces results in a fractional number, a fraction of less than .5 shall be disregarded and a fraction of .5 or more shall be rounded to the next highest whole number.

#### **8.1.3. DRIVEWAY AND PARKING SPACE REQUIREMENTS FOR SINGLE-FAMILY AND DUPLEX DWELLINGS ON INDIVIDUAL LOTS.**

**A.** New Single-Family, Duplex, and Triplex dwellings shall construct and maintain a paved area large enough to accommodate two (2) 9'x18' off-street parking spaces per dwelling unit (excluding garage spaces) unless the public street on which the driveway connects is 20 feet wide or less of paved surface (excluding curb and gutter). In such cases, three (3) 9'x18' paved off-street parking spaces per dwelling unit shall be constructed and maintained. The driveway and garage/carport can serve as these spaces. The requirements of Appendix D shall also apply to the construction of driveways connecting to public streets.

**B.** Parking is permitted in the front yards of single family, duplex, or triplex dwellings in any zoning district other than agricultural. Front yard

parking areas are subject to the following requirements:

1. No person shall park or store any motor vehicle in the front yard (between the street and a line drawn parallel to the street from the point of the dwelling that is closest to the street) other than completely upon an improved driveway or improved parking pad.
2. Improved drive and parking areas shall be limited in size to eighteen (18) feet by eighteen (18) feet and shall not exceed thirty percent (30%) of the structure's front yard. Access to the improved front yard area shall be limited to properly approved curb cuts or other approved access points. Proposed improved parking areas shall be designed and installed so as to avoid creating standing water conditions, diverting runoff onto neighboring properties, or adversely impacting stormwater water quality.
3. Improved parking or vehicular use areas shall be surfaced with asphalt, concrete, brick, or other pavers as approved by the Planning Director. Gravel or crushed stone may be used if the gravel is at least two (2) inches deep throughout the vehicular use area, and the vehicular use area has a visible and definable edge made of landscape timbers, vegetation such as low shrubs or decorative grasses, or similar technique to distinguish the vehicular use area from the front or corner side yard area.

## 8.2. <sup>(1)</sup>PRIVATE DRIVEWAY PROVISIONS.

### 8.2.1. SCOPE.

All proposed vehicular access points to connect to a public street shall conform to the Access Management provisions of this Section 8.2, as well as applicable sections of Article 10 and Appendix D of this Ordinance. This Section 8.2 shall apply to all driveways or access points to be maintained on private property. The provisions of Appendix D of this Ordinance shall regulate that portion of a private driveway that connects to a public and extends to the edge of a public right-of-way.

**A.** If ingress and egress are the same, off-street parking spaces shall be connected to a public street by a paved driveway which affords safe and convenient ingress and egress provided, however, that the Administrator may waive this requirement where:

1. the driveway is connected to an adjacent driveway or series of driveways with access to a public street, and
2. the applicant has a valid easement providing for access to all driveways leading to the public street.

**B. Driveway Width** - The width of driveways shall be measured at the point of intersection with the public street right-of-way. Driveway width shall be regulated in accordance with Table 8.2-1 below, unless the Administrator determines the width should be expanded as set forth in § 8.2.1(C). Medians shall not be included in the calculation for the width of driveways. Where no right-of-way exists, the Administrator shall determine the most appropriate location for the measurement.

**C.** The Administrator may waive the requirements of § 8.2.1(B) only under the following conditions:

1. The Public Works Director determines that a wider turning area is needed in order to avoid a traffic hazard,
2. The Public Works Director and the Administrator jointly determine an appropriate distance from the point of intersection with the public street right-of-way where the driveway shall conform to the dimensional requirements of Table 8.2-1,
3. The design of the driveway is such that it progressively decreases in width to conform to the width as determined in Table 8.2-1,
4. Review and final approval of any proposed driveway design that does not conform to the dimensional limitation shall be under the authority of the Administrator upon recommendation from the Public Works Director.

**Table 8.2-1 – Driveway Width**

Type	Min.	Max.
One –way	*	20 feet
Two-way	*	36 feet

\*subject to parking design as set forth in Column “D” of Table 8.3-2

### 8.3. <sup>(2)</sup>OFF-STREET PARKING STANDARDS.

#### 8.3.1. REQUIRED AMOUNT OF OFF-STREET PARKING.

**A.** Off-street parking spaces shall be provided in accordance with the requirements of Table 8.3-1.

**B. Uses With Variable Parking Demand Characteristics** – Uses that reference this subsection in Table 8.3-1 have widely varying parking and loading demand characteristics, making it difficult to establish a single off-street parking or loading standard. Upon receiving a development application for a use subject to this subsection, the Planning Director shall apply the off-street parking and loading standard specified or listed use that is deemed most similar to the proposed use, or establish minimum off-street parking standards on the basis of a parking and loading demand study prepared by the applicant. Such a study shall include estimates of parking demand based on recommendations of the Institute of Traffic Engineers (ITE) or other acceptable estimates as approved by the Planning Director, and should include other reliable data collected from uses or combinations of uses that are the same as or comparable with the proposed use. Comparability will be determined by density, scale, bulk, area, type of activity, and location. The study shall document the source of data used to develop the recommendations.

**C. Alternative Parking Plans** – An Alternative Parking Plans represents a proposal to meet vehicle parking and transportation demands by means other than providing parking in accordance with the requirements of Table 8.3-1. Alternative Parking Plans may not be used to reduce required setbacks, landscaping, or screening of off-street parking areas.

**1. Applicability** – Applicants who wish to provide fewer off-street parking spaces than required pursuant to Table 8.3-1 shall be required to secure approval of an Alternative Parking Plan in accordance with the standards of this Section.

#### **2. Review and Approval**

**a.** The Planning Director shall be authorized to approve, approve with conditions, or deny approval of Alternative Parking Plans for developments that will provide at least seventy-five percent (75%) of the required number of off-street parking spaces.

**b.** All other Alternative Parking Plans shall require review and approval by the Planning & Zoning Commission. In order to approve an Alternative Parking Plan, the Planning & Zoning Commission must determine that the proposed plan will do at least as good a job protecting surrounding neighborhoods, maintaining traffic circulation patterns, and promoting quality urban design as would strict compliance with otherwise applicable off-street parking standards.

**3. Contents** – Alternative Parking Plans shall, at a minimum, address the size and type of the proposed development, the composition of tenants, the anticipated rate of parking turnover, and the anticipated peak parking and traffic loads for all uses.

**4. Recording** – Approved Alternate Parking Plans shall be recorded in the office of the County Register of Deeds. No certificates of compliance shall be issued until proof of recordation of the agreement has been provided to the Planning Director.

**D. Parking Structures Exempted.** The maximum parking requirements shall not apply to parking spaces within an above-ground or an underground parking structure. For the purposes of parking calculations, the gross area of any parking garage within a building shall not be included within the Gross Floor Area of the building.

**E. <sup>(1)</sup>Exemption from Required Number of Spaces** - The minimum requirement for the number of vehicle parking spaces shall not apply within the following zoning districts:

TND Traditional Neighborhood Development  
TOD Transit-Oriented Development

**F. Uses Not Identified in Table 8.3-1** The Administrator shall determine the parking requirement for uses which do not correspond to the categories listed in Table 8.3-1. In such instances, the applicant shall provide adequate information by which the proposal can be reviewed, which includes but may not necessarily be limited to the following:

- type of use(s);
- number of employees;
- the Occupant Load (per Building Code) of the building;
- square feet of sales area and service area;
- parking spaces proposed on-site;
- parking spaces provided elsewhere; and
- hours of operation.

**G. Multiple Uses** - In those instances where there are clearly identified accessory or multiple uses within a structure or multiple structures, the minimum standards shall apply to each use or structure, resulting in a total parking requirement when summed, except as provided in § 8.3.1(I).

**H. Seating Calculations.** When seating consists of benches, pews, or other similar seating facilities, each 20 linear inches of seating space shall be counted as 1 seat. Where parking requirements relate to movable seating in auditoriums and other assembly rooms, 15 square feet of net floor area shall be construed to be equal to one seat.

**1. Modification to Required Number of Spaces.** In unusual circumstances, the standard parking requirement may not be appropriate. The Administrator shall have the authority to reduce the parking requirement by up to 10 percent, if fifty (50%) of the reduced area is used for parking lot landscaping. Additional landscaping must be in addition to that required by Article 7 of this Ordinance. Bonus landscaping area for reduced parking spaces shall only apply to industrial, commercial, and office/institutional districts.

**I. Shared Parking** - Off-street parking requirements of a given use may be met with off-site, off-street parking facilities of another use when, and if, all of the following conditions are met:

**1.** In non-residential zoning districts, the parking may be up to five hundred (500) feet from the principal structure;

**2.** The applicant shall submit a shared parking study to the Planning Director clearly demonstrating the feasibility of shared parking. The study shall address, at a minimum, the size and type of proposed development and/or use, the composition of tenants, the anticipated rate of parking turnover, and the anticipated peak parking and traffic loads for all uses that will be sharing off-street parking spaces; and

**3.** A written agreement between the owners and lessees is executed for a minimum of ten (10) years, approved by the Administrator, recorded, and a copy maintained in the project file. Should the lease expire or otherwise terminate, the use for which the off-site parking was provided shall be considered to contain nonconforming site improvements. Future expansion of the use shall be prohibited unless the use is brought into compliance with the parking regulations of this Ordinance.

**J.** Developments which contain a mix of uses on the same parcel, as set forth in Table 8.1-5 below, may reduce the amount of required parking in accordance with the following methodology: (1) determine the minimum parking requirements in accordance with Table 8.3-1 for each land use as if it were a separate use, (2) multiply each amount by the corresponding percentages for each of the five time periods set forth in Columns (B) through (F) of Table 8.3-3, (3) calculate the total for each time period (Columns), (4) select the Column with the highest total, and (5) use this number as the required minimum number of parking spaces.

Minimum dimensions of standard parking spaces (other than compact car spaces and handicap spaces) and maneuvering area shall be as set forth in Table 8.3-2.

### **8.3.2. PARKING AISLE AND SPACE DIMENSIONS.**

**A. Handicapped Spaces** – Handicapped parking spaces shall be a minimum of 13 feet

by 18 feet for a single non-van space (8 feet in width in addition to a 5 foot access aisle); a minimum of 16 feet by 18 feet for a single van space (8 feet in width in addition to an 8 foot access aisle); or 24 feet by 18 feet for a double van space, or a non-van and van double space (8 feet in width for each space with an 8 foot access aisle between spaces). Parking spaces for handicapped or disabled persons shall comply with Chapter 4 of the North Carolina Accessibility Code. The required number of handicapped spaces is depicted in Table 8.3-4.

**B. Compact Car Spaces** – This section shall apply only to retail shopping centers/superstores of 25,000 square feet or more. Not more than twenty (20) percent of the off-street parking spaces required by this Section shall be designed as compact car parking spaces. Each compact car parking space shall be a minimum of eight (8) feet wide and sixteen (16) feet long. Compact car parking spaces shall be clearly marked or posted for “Compact Cars Only.” All other provisions of this Article relating to off-street parking requirements shall be met.



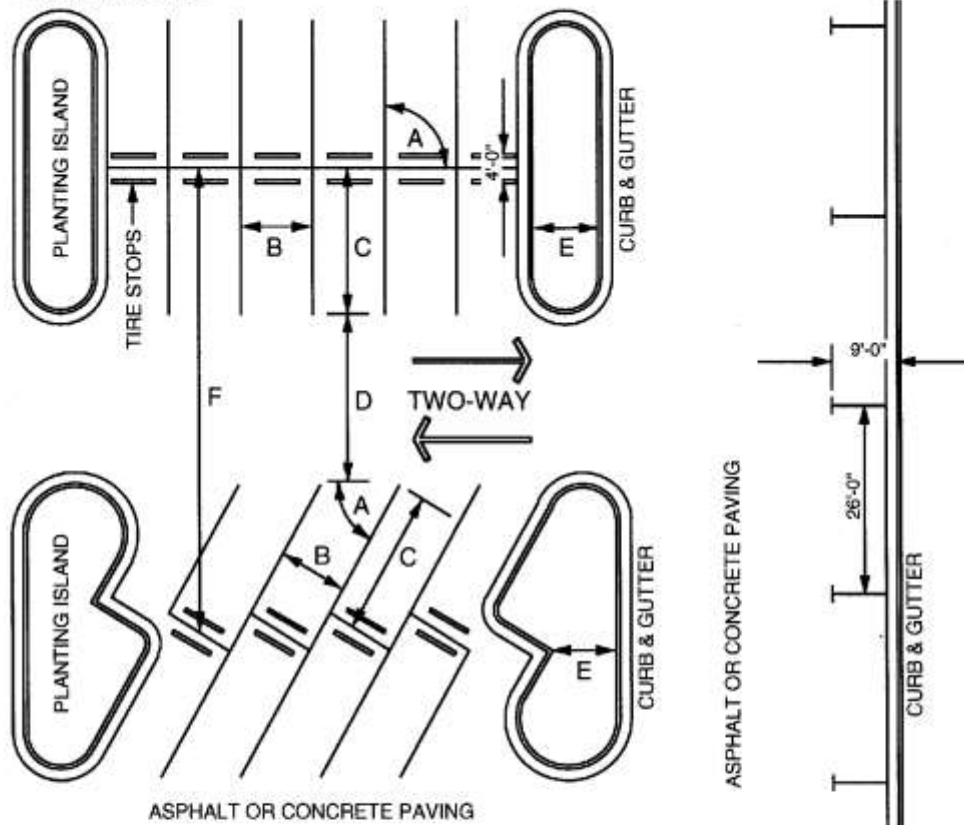
<b>Table 8.3-1 Off-street Vehicular Parking Space Requirements</b>		
<b>Land Use</b>	<b>Min. Vehicle Space</b>	<b>Max Vehicle Space</b>
<b><i>Residential</i></b>		
Accessory Dwellings	1 per unit	n/a
Duplex/Triplex	2 per unit	n/a
Family Care or Group (Social Assistance) Home	1 per employee + 1 per every 5 children or 1 per every 3 adults	n/a
Multi-Family (4 or more units) and attached Single-Family	1.5 per 1-2 bedroom unit + 2 per 3+ bedroom unit	2.5 per unit
①Elderly or Age Restricted Multi-Family (4 or more units)	1 per dwelling unit + 1 per employee	n/a
Single Family, detached (includes manufactured and modular homes)	2 per parcel (excluding garage spaces)	n/a
Nursing Home	1.5 per employee + 0.33 per bed	n/a
<b><i>Institutional and Civic</i></b>		
Auditorium/Public Assembly	Use with Variable Parking Demand Characteristics	n/a
Botanical Garden and Nature Preserves	Use with Variable Parking Demand Characteristics	n/a
Campground	1 per 6 camp sites + 4 per laundry & shower facility	n/a
Child Care Center	1.5 per classroom + 1 per 300 sf office/administration space + sufficient stacking area for drop-off / pick-up	2.5 per classroom + 1 per 150 sf office/administration space + sufficient stacking area for drop-off / pick-up
Civic, Social and Fraternal Organizations	1 per 250 sf GFA	1 per 200 sf GFA
Emergency Service Facilities	1 per employee + 1 per 3 volunteer personnel on normal shift + 1 per 200 sf usable office space	n/a
Golf Course	4 per hole + 1 per employee at peak shift	1 per tee + per employee at peak shift
Golf Driving Range	1 per 2 tees + 1 per employee at peak shift	1 per tee + 1 per employee at peak shift
Government Buildings	1 per 300 sf GFA	1 per 125 sf GFA
Hospital	1 per 400 sf GFA	1 per 100 sf GFA
Museums and Art Galleries	1 per 1,000 sf GFA + 1 employee at peak shift	1.5 per 1,000 sf GFA + 1 employee at peak shift
Park, public	Use with Variable Parking Demand Characteristics	n/a
Religious Institutions	1 per 8 seats	1 per 1.5 seats
Schools – Business, Technical, Trade and/or other Vocational	1 per 2 students + 1 per 300 sf office/administration space	1 per student + 1 per 300 sf office/administration space
Schools – Elementary and Middle	1 per classroom + 1 per 300 sf office/administration space + sufficient stacking area for drop-off / pick-up	2 per classroom + 1 per 300 sf office/administration space + sufficient stacking area for drop-off / pick-up
Schools – Fine Arts	1 per 150 sf GFA + sufficient stacking area for drop-off / pick-up	1 per 50 sf GFA + sufficient stacking area for drop-off / pick-up
Schools – Senior High	1 per classroom + 1 per 300 sf office/administration space + 1 per every 6 students + sufficient stacking area for drop-off / pick-up	2 per classroom + 1 per 300 sf office/administration space + 1 per every 4 students + sufficient stacking for drop-off / pick-up

Schools – University or College	Use with Variable Parking Demand Characteristics	n/a
Zoo	Use with Variable Parking Demand Characteristics	n/a
All other Institutional Uses	1 per 300 sf GFA of bldg..	1 per 125 sf of bldg..
<b>Professional Office / Business Services</b>		
Automobile Repair/Body Shop	1 per 500 sf GFA including all service areas + 1 per employee	1 per 375 sf GFA including all service areas + 1 per employee
Banks (with drive-through)	1 per 200 sf GFA, plus sufficient stacking spaces to accommodate any drive-through lane(s)	1 per 150 sf GFA, plus sufficient stacking spaces to accommodate any drive-through lane(s)
Banks (without drive through)	1 per 250 sf GFA	1 per 140 sf GFA
Contractor's Office	1 per 600 sf GFA	n/a
Funeral Home and Services	1 per fixed seats or 1 per 75 sf of parlor or chapel area, whichever is greater	1 per 2 seats or 1 per 50 sf parlor or chapel area, whichever is greater
Personal Services (Dry Cleaners, etc.)	1 per 500 sf GFA	1 per 215 sf GFA
Personal Care Services (Hair, skin, etc.)	1 per 500 GFA	1 per 215 sf GFA
All other Professional Office / Business Service Uses	1 per 300 sf GFA	1 per 150 sf GFA
<b>Retail Trade</b>		
Amusement Arcade (Indoor Only)	1 per game table, video game or other amusement device	n/a
Amusement Park	Use with Variable Parking Demand Characteristics	n/a
Automobile Rental and Leasing	1 per 300 sf GFA + 1 per employee + 1 per vehicle stored on site	1 per 150 sf GFA + 1 per employee + 1 per vehicle stored on site
Automobile Sales, New and Used (see § 5.6)	1 per 375 sf GFA of sales area + 1 per employee + 1 per vehicle stored on site	1.5 per 375 sf GFA of sales area + 1 per employee + 1 per vehicle stored on site
Bar/Nightclub or Micro-brewery	1 per 4 seats	1 per 2 seats
Bed and Breakfast Inns (see § 5.7)	1 per guest bedroom + 2 for owner/operator	n/a
Bowling Centers	2 per lane + 1 per employee at peak shift	4 per lane + 1 per employee at peak shift
Building Material Supply / Greenhouses / Nurseries / Lawn & Garden Supply	1 per 375 sf of sales or service building area	1.5 per 375 sf of GFA of sales or service building area
Car Wash (as a principal use), Truck Stops, and/or Travel Plaza	1 per 500 sf GFA including wash bays/tunnels and retail area	1 per 375 sf GFA including wash bays/tunnels and retail areas
Convenience Store	6 per 1,000 GFA + sufficient stacking area to accommodate 2 vehicles per each side of pump island	10 per 1,000 GFA sufficient stacking are to accommodate 2 vehicles per each side of pump island
General Merchandise Stores (less than 25,000 sf GFA)	1 per 300 sf GFA	1 per 200 sf GFA
Grocery / Food Stores	1 per 300 sf GFA	1 per 200 sf GFA
Health Clubs and Fitness Centers	1.5 per 1,000 sf GFA	10 per 1,000 GFA
Hotels, Motels and Extended Stay Facilities	1 per room + 1 per employee at peak shift + spaces to accommodate all accessory uses	2 per room + 1 per employee at peak shift + spaces to accommodate all accessory uses
Manufactured Home Sales	1 per 100 sf GFA of office area + 1 per every 2 employees	n/a
Miniature Golf	1 per hole + 1 per employee at peak shift	2 per hole + 1 per employee at peak shift

Mini-warehousing / Self-storage Leasing (see § 5.15)	1 for every 200 rental spaces (2 required)	n/a
Motion Picture Theater	1 per 6 seats	1 per 4 seats
Motion Picture Theater, drive-in	1 for each viewing space the facility is designed to accommodate + 1 per every 2 employees	1 for each viewing space the facility is designed to accommodate + 1 per every employee
<sup>(1)</sup> Pharmacies	1 per 300 sf GFA	n/a
Pool Halls / Billiard Parlors	2 per table + 1 per employee at peak shift	3 per table + 1 per employee at peak shift
Restaurants (with drive-through service and/or carry-out)	1 per every 3 seating accommodation + sufficient stacking area to accommodate any drive-through lane(s)	1 per 50 sf GFA + sufficient stacking area to accommodate any drive-through lane(s)
Shopping Centers/Superstores (25,000 – 400,000 sf GFA)	1 per 205 sf GFA	1 per 150 sf GFA
Shopping Centers / Superstores (400,001 – 600,000 sf GFA)	1 per 225 sf GFA	1 per 150 sf GFA
Shopping Centers / Superstores (over 600,000 sf GFA)	1 per 200 sf GFA	1 per 150 sf GFA
All other Retail uses	1 per 300 sf GFA	1 per 150 sf GFA
<b><i>Wholesale Trade uses, Manufacturing and Industrial Uses</i></b>		
Junk / Salvage Yard	1 per 2 employees at peak shift + 1 per 5,000 sf of land area devoted to material storage + 1 per company vehicle at peak shift	n/a
Manufacturing uses	2 per 3 employees at peak shift + 1 per each company vehicle at peak shift	n/a
Mining and Extractive Uses	1 per employee at max. shift + 1 per each company vehicle at peak shift	n/a
Wholesale sales and all other Industrial Uses	1 per 400 sf GFA of sales and office area + 1 per each company vehicle at peak shift OR 2 per 3 employees at peak shift + 1 per each company vehicle at peak shift (whichever is greater)	n/a
<b><i>Transportation, Warehousing and Utility Uses</i></b>		
Air, Rail or Bus Terminal	Use with Variable Parking Demand Characteristics	n/a
Communications Tower / Antenna	None (see § 5.21)	n/a
Truck Terminal	1 per employee at peak shift + 1 per each company vehicle at peak shift	n/a
Warehouse and Storage	1 per 400 sf GFA of sales and office area + 1 per each company vehicle at peak shift OR 2 per 3 employees at peak shift + 1 per each company vehicle at peak shift (whichever is greater)	n/a
Utility Uses	1 per employee at peak shift + 1 per each company vehicle at peak shift	n/a

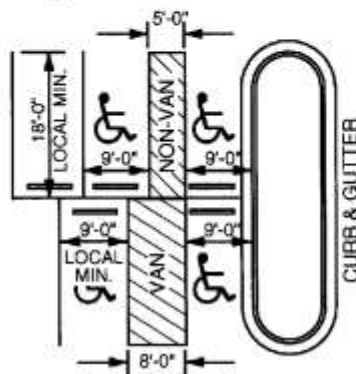
**FIGURE 8.3-1  
STANDARD PARKING DIMENSIONS**

**Standard Parking  
Dimensions**



**Handicapped Parking  
Dimensions**

See North Carolina  
Accessibility Code



**TABLE 8.3-2 <sup>(1)</sup>**  
**PARKING AREA DIMENSIONAL STANDARDS**

<b>Standard-size Vehicles</b>						
<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>
0°	9.0'	26.0'	12.0' ONE-WAY	N/A	N/A	N/A
45°	9.0'	18.0'	12.0' ONE-WAY	6.3'	51.0'	2.0' / 4.0'
60°	9.0'	18.0'	18.0' ONE-WAY	7.8'	58.0'	2.0' / 4.0'
90°	9.0'	18.0'	24.0' TWO-WAY	9.0'	60.0'	2.0' / 4.0'

<b>Compact-size Vehicles</b>						
<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>
45°	8.0'	16.0'	N/A	5.3'	N/A	1.5' / 3.0'
60°	8.0'	16.0'	N/A	6.8'	N/A	1.5' / 3.0'
90°	8.0'	16.0'	N/A	8.0'	N/A	1.5' / 3.0'

Key (also see Figure 8.3-1):

- A. Parking Angle
- B. Stall Width
- C. Stall Depth
- D. Aisle Width
- E. Planting Island Width (minimum width shown, refer to Article 7 for minimum area)
- F. Parking Bay Width
- G. Bumper Overhang (front / rear)

**TABLE 8.3-3**  
**SHARED PARKING ALLOWANCES BY LAND USE**

A	B	C	D	E	F
	Weekday		Weekend		
Land Use	Daytime*	Evening*	Daytime*	Evening*	Nighttime*
Office/Industrial	100%	10%	10%	5%	5%
Retail	60%	90%	100%	70%	5%
Hotel	75%	100%	75%	100%	75%
Restaurant	50%	100%	100%	100%	10%
Entertainment/ Commercial	40%	100%	80%	100%	10%

\*Key:

- Daytime (6am – 5pm)
- Evening (5pm – midnight)
- Nighttime (midnight – 6 am)

**TABLE 8.3-4  
HANDICAPPED PARKING SPACES REQUIRED**

<b>Total parking spaces provided</b>	<b>1-25</b>	<b>26-50</b>	<b>51-75</b>	<b>76-100</b>	<b>101-150</b>	<b>151-200</b>	<b>201-300</b>	<b>301-400</b>	<b>401-500</b>	<b>501-1000</b>	<b>1001 &amp; over</b>
<b>Minimum number of accessible spaces required</b>	1	2	3	4	5	6	7	8	9	2% of total	20+ 1 for each 100 over 1000
<b>Minimum Number of accessible spaces required to be van accessible (see note)</b>	1	1	1	1	1	1	1	1	2	1 in every eight accessible spaces	1 in every eight accessible spaces

### **8.3.3 VEHICLE STACKING AREAS**

The vehicle stacking standards of this subsection shall apply unless otherwise expressly approved by the Planning Director.

#### **A. Minimum Number of Spaces.**

- 1.** Off-street stacking spaces shall be provided as required in Table 8.3-5.
- 2.** Required stacking spaces are subject to the following design and layout standards.
  - a. Size** – Stacking spaces shall be a minimum of 10 feet by 20 feet in size.

**TABLE 8.3-5  
VEHICLE STACKING SPACES REQUIRED**

<b>ACTIVITY TYPE</b>	<b>MINIMUM NUMBER OF SPACES</b>	<b>SPACES MEASURED FROM</b>
Automated Teller Machine	3	Teller Machine
Bank Teller Lane	4	Teller or Window
Restaurant Drive-Thru	4	Order Box
Restaurant Drive-Thru	4	Order Box to Pick-up Window
Quick Lube	3	Entrance
Daycare Facility	5	Drop-off Entrance
Schools	10	Drop-off Entrance
Hospital	5	Drop-off Entrance
Car Wash Stall (Self-service)	3	Entrance
Car Wash Stall (Automatic)	4	Entrance

## 8.4. OFF-STREET LOADING AND UNLOADING AREA STANDARDS.

### 8.4.1. SCOPE.

There shall be provided on the same lot with each nonresidential building or structure, adequate space for off-street loading, unloading and the maneuvering of shipping and delivery vehicles. Off-street maneuvering space shall be provided so that no backing onto or from a public street is required. All loading and maneuvering areas shall:

- be surfaced with pavement, concrete or equivalent,
- be properly drained,
- be designed with regard to pedestrian safety,
- have direct access to public streets, and
- shall be screened from adjacent residentially zoned or developed property as provided in Article 7 Landscaping Standards of this Ordinance.
- No loading docks shall be visible from a thoroughfare or collector street right-of-way.

### 8.4.2. REQUIRED AMOUNT.

A. The Uses required to provide off street loading/unloading space and, the quantity and size of said space required shall be:

**1. Retail and service establishments -** One (1) loading space with minimum dimensions of 12 feet by 25 feet for every 20,000 square feet of Gross Leasable Area rounded off to the nearest 20,000.

**2. Office buildings and lodging establishments -** One (1) loading space with minimum dimensions of 12 feet by 25 feet for every 50,000 square feet of Gross Leasable Area rounded to the nearest 50,000.

**3. Industrial/manufacturing and wholesale establishments** shall provide the following loading spaces with a minimum dimension of 12 feet by 25 feet:

- Up to 50,000 sf 1 space
- 50,000 120,000 sf 2 spaces
- 120,000 220,000 sf 3 spaces
- 220,000 350,000 sf 4 spaces
- 350,000 550,000 sf. 5 spaces
- 550,000 850,000 sf. 6 spaces
- Each additional 400,000 sf 1 additional

space

**B.** Off street loading/unloading areas shall be located such that interference with traffic on Streets is minimized (subject to approval by the Administrator).

**C.** No off street loading/unloading space shall be sized such that any reasonably anticipated vehicle utilizing the space will protrude into any required Parking Space and/or Street right of way.